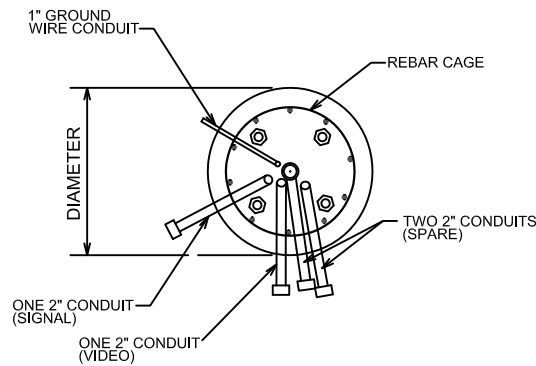
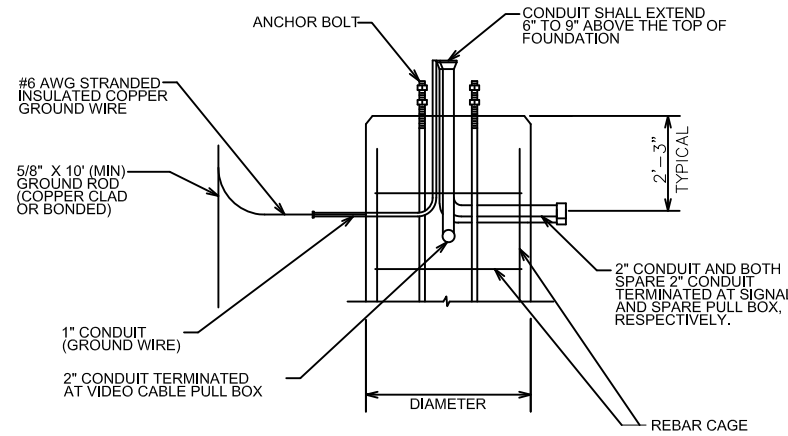


FOUNDATION CONDUIT DETAIL



PLAN



PROFILE

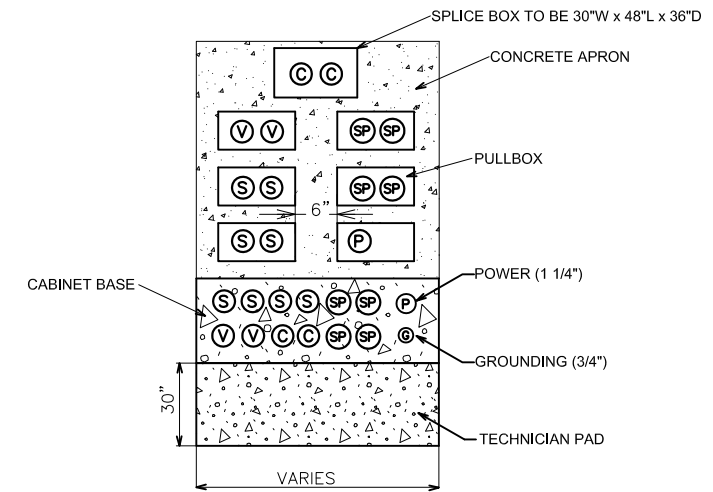
NOTE:
 * 3" SIGNAL CONDUIT IS REQUIRED WHEN 2 SIGNAL CABLES ARE BEING RUN IN THE SAME CONDUIT.
 THIS CONFIGURATION IS ONLY PERMITTED WITH DUAL MAST ARMS.

DIRECTION OF CONDUITS EXITING THE SIDE OF THE FOUNDATION IS SHOWN ONLY TO ILLUSTRATE DIMENSIONS MORE CLEARLY.

DIRECTION/BUNDLES OF CONDUITS EXITING THE SIDE OF THE FOUNDATION COULD VARY FROM WHAT IS SHOWN IN THE DETAILS IN ORDER TO AVOID ANCHOR BOLTS AND REBAR.

WHEN PRE-EMPTION IS REQUIRED, AN ADDITIONAL 2" CONDUIT SHALL BE INSTALLED.

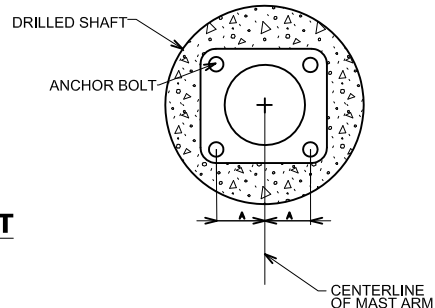
PULL BOX CABINET CONDUIT DETAILS



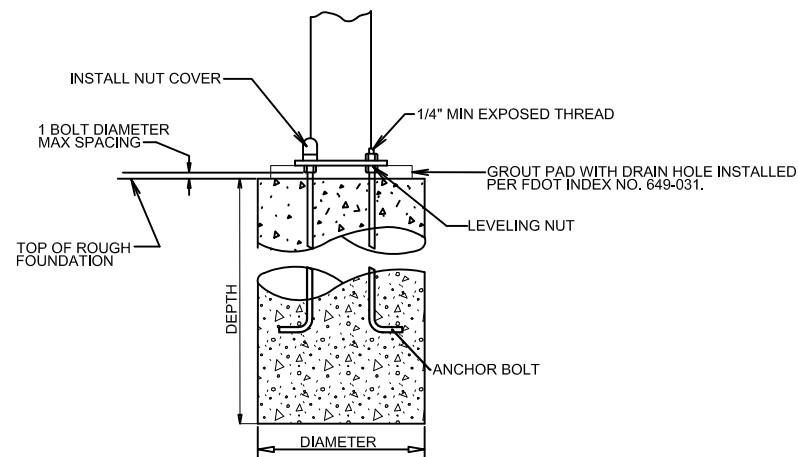
- LEGEND**
- (S) SIGNAL
 - (V) VIDEO
 - (C) COMMUNICATION
 - (SP) SPARE
 - (P) POWER
 - (G) GROUND
 - [Concrete symbol] CONCRETE

NOTE:
 ALL CONDUIT SHOWN SHALL BE 2" IN DIAMETER, UNLESS OTHERWISE SPECIFIED.

UPRIGHT BASE PLACEMENT ON DRILLED SHAFT



UPRIGHT TO DRILL SHAFT CONNECTION DETAIL



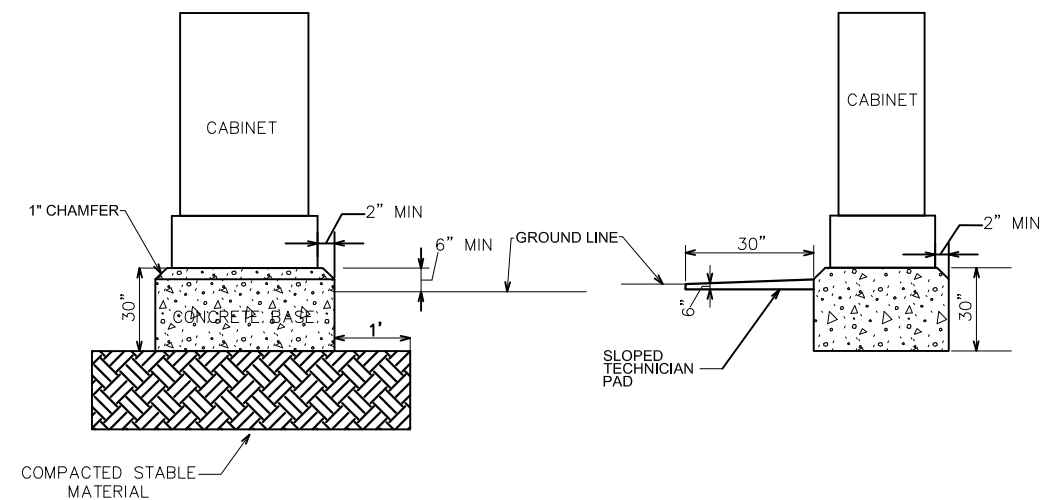
CABINET INSTALLATION NOTES

1. TOP OF CABINET BASE MUST BE AT LEAST 6" ABOVE THE FINISHED GRADE.
2. FOLLOW FDOT INDEX 676-010 FOR ANY ADDITIONAL DETAILS.
3. CABINET BASE SHALL BE OF CLASS 1, 3000 PSI CONCRETE.
4. CONDUITS SHOWN ARE THOSE TO BE INCLUDED FROM PULL BOX TO CABINET.
5. PULL BOXES SHOULD BE AT THE REAR OR SIDE OF THE CABINET. DIFFERENT ORIENTATION WILL BE ALLOWED IF SUFFICIENT RIGHT OF WAY IS UNAVAILABLE.
6. CABINET SHALL BE TS2 TYPE 1 - TYPE VI SIZE. A REDUCED DEPTH TYPE VI CABINET OR APL LISTED ALTERNATIVE MAY BE APPROVED BY TRAFFIC ENGINEERING DIVISION IF THERE IS LIMITED RIGHT OF WAY.
7. CONTRACTOR SHALL COMPACT MATERIAL UNDER CABINET BASE TO 95% AASHTO T-99 DENSITY. COUNTY INSPECTOR MAY TAKE DENSITIES TO CHECK COMPACTION.
8. CONSTRUCT CONCRETE APRON PER FDOT STANDARD INDEX 635-001.
9. CONTROLLER CABINET SHALL BE ORIENTED SUCH THAT CABINET DOOR OPENS AWAY FROM THE INTERSECTION AND SHALL HAVE SUNSHIELDS ON THE TOP AND SOUTH FACES.
10. FINAL LOCATIONS OF ALL CABINETS SHALL BE APPROVED BY ENGINEER PRIOR TO PLACEMENT OF FOUNDATION IF THE LOCATION HAS CHANGED FROM THE PLAN.

DRILLED SHAFT NOTES

1. DRILLED SHAFT CONSTRUCTION SHOULD BE IN ACCORDANCE WITH SECTION 455 OF FDOT SPECIFICATIONS.
2. REFER TO AUGUST 2007 UNION METAL DRAWING 50300-B994 FOR MAST ARM ASSEMBLY CONNECTION DETAILS.
3. SEE FDOT INDEX NO. 649-031 AND MAST ARM ASSEMBLIES SHEET OF CONTRACT PLANS FOR THE REBAR CAGE FOR THE DRILLED SHAFT AND ANY DETAILS NOT SHOWN.
4. COUNTY WILL PERMIT UP TO SIX (6) INCHES OF THE DRILLED SHAFT FOUNDATION TO BE ABOVE EXISTING GRADE. THE DRILLED SHAFT FOUNDATION SHALL BE FLUSH IF IN OR ADJACENT TO SIDEWALK. THE DRILLED SHAFT FOUNDATION DEPTH AND THE LENGTH OF STEEL REINFORCEMENT REBAR WILL BE INCREASED ACCORDINGLY TO MEET INDEX 649-031 AND THE MAST ARM ASSEMBLIES SHEET.
5. DRILLED SHAFT CONCRETE SHALL BE CLASS IV WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI.
6. ORIENT ALL CONDUIT AS REQUIRED TO AVOID ANCHOR BOLTS AND REBAR IN THE FOUNDATION.
7. BOLT PATTERN AND ITS ORIENTATION IS THE RESPONSIBILITY OF THE CONTRACTOR. ANCHOR BOLTS MUST BE SET SO THAT UPRIGHT IS CENTERED ON THE DRILLED SHAFT FOUNDATION.
8. CORRUGATE CASING IS NOT PERMITTED FOR THE CONSTRUCTION OF DRILLED SHAFTS.
9. LEVELING NUTS MUST HAVE BOTTOM WASHERS.

CABINET PAD DETAIL



REVISIONS

DATE	DESCRIPTION
04-21-2016	N/A
08-03-2017	N/A
06-14-2018	UPDATED FDOT INDEX REFERENCE



**PUBLIC WORKS DEPARTMENT
 TRAFFIC ENGINEERING DIVISION**

DESIGN BY: REBECCA MARTINEZ, P.E. SCALE: NTS
 DRAWN BY: STEPHON RAMOUTAR
 CHECKED BY: YVES D'ANJOU, P.E.

**DRILLED SHAFT FOUNDATION
 AND CABINET DETAILS**

SHEET NO.

1 OF 1